



STREETSCAPE AND DECK-OVER PROJECT

FROM NORTH OF DUPONT CIRCLE TO CALIFORNIA STREET NW

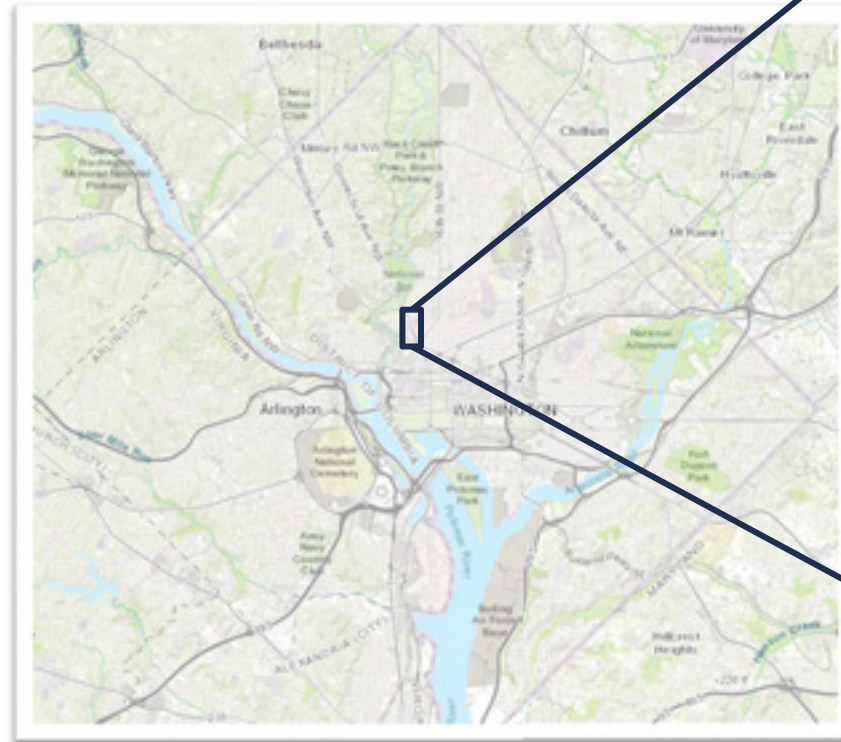
Project Update *November 2021*

Project Overview



Project Location

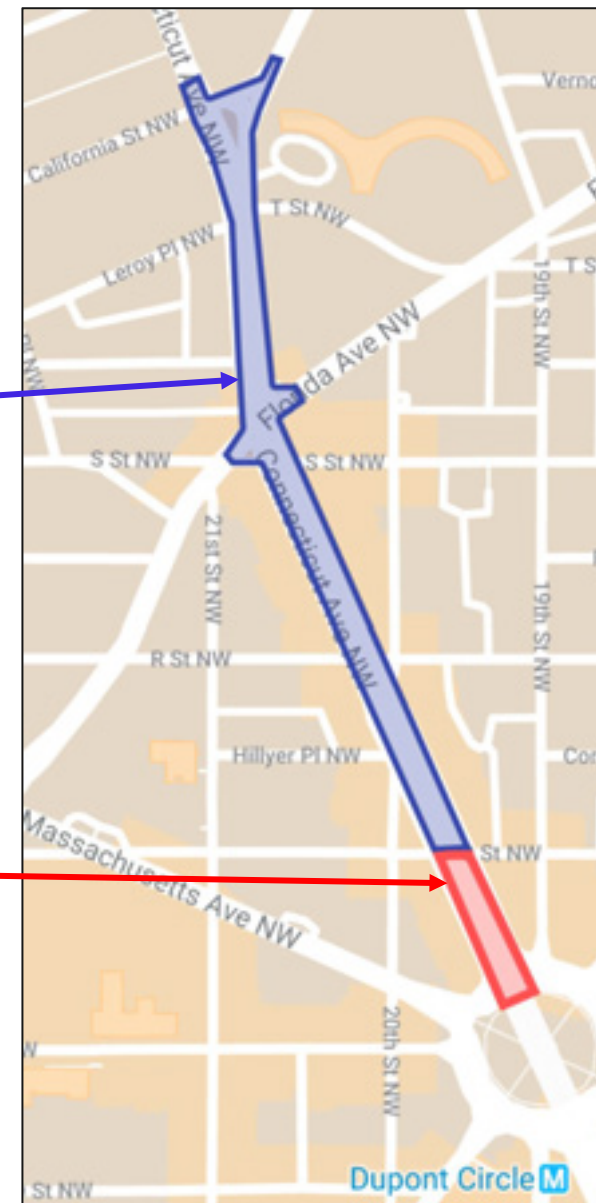
- Connecticut Avenue NW
 - Streetscape limits: Q Street NW to California Street NW
 - Deck-over (Plaza) limits: North of Dupont Circle to Q Street NW



Project Overview

Project Goals

- **Develop a Productive, Sustainable Space & Streetscape**
- Streetscape Improvements
 - Support safe and effective multimodal access for pedestrians, bicyclists, transit, and automobiles
 - Upgrade sidewalks, streetlights, landscape, drainage, and traffic signals
 - Provide new protected bicycle facilities
- New Deck-over Plaza
 - Create a new public space for the community for strolling, relaxing, working, and other future programming



CFA Comments from January 2021

- **Approved Streetscape Improvements from Q Street to California Street**
 - Requested consideration for not using brightly colored paint on pavement for bus lanes and bike lanes
- Supported the creation of the pedestrian plaza
- Raised concerns about the plaza design
 - Encouraged continuous street tree placement along plaza
 - Regular placement of the honey locusts in planters
 - Supported oval plants and playfulness of varying sizes
 - Recommended stone in place of precast concrete boulders or use of more regular and substantial design element to avoid future installation of supplemental barriers
- Requested Resubmission of Plaza portion of project



U.S. COMMISSION OF FINE ARTS

ESTABLISHED BY CONGRESS 17 MAY 1910

401 F STREET NW SUITE 501 WASHINGTON DC 20001-2726 202-644-1234 FAX 202-644-2742 WWW.CFA.DC

28 January 2021

Dear Mr. Lott:

In its public meeting of 21 January conducted by videoconference, the Commission of Fine Arts reviewed a concept design for proposed streetscape improvements along Connecticut Avenue, NW, between Dupont Circle and California Street. The Commission approved the concept for the northern portion of the project, from California Street to Q Street; for the southern portion of the project from Q Street to Dupont Circle, which includes a new plaza atop the open vehicular underpass, the Commission took no action and requested a further concept submission that responds to the following comments.

The Commission members expressed support for the creation of a pedestrian plaza to animate the block of Connecticut Avenue immediately north of Dupont Circle, but they raised numerous concerns about the proposed design. Commenting that the relationship of this block to Dupont Circle is critical to conceptualizing the design, they requested that the plan drawings include the open space, landscape, and fountain of Dupont Circle for reference. They encouraged the continuous placement of street trees without interruption at the pedestrian crosswalk; for the honey locust trees proposed in planters located within the plaza, they suggested that these be as regular as possible to reinforce the visual connection to the open space of Dupont Circle, possibly with additional trees at the north and south ends of the plaza. They acknowledged the advantages of the planters' oval shape in providing increased soil capacity within this narrow corridor, while expressing some support for the playfulness of the proposed configuration of various sizes and alignments, they also requested that the next submission include an alternative design with a more regular configuration of planters. Acknowledging the intent to protect pedestrians from vehicles, they recommended using natural stone instead of precast concrete for the curbside boulders, or using a more regular and substantial design element, they urged the development of a design that will not require the future addition of unsightly supplemental barriers.

In their support for the overall streetscape project, the Commission members observed that bicycle and bus lanes in Washington are often painted in bright colors that can be jarring within the urban context. They encouraged the D.C. Department of Transportation's citywide reconsideration of the standard design treatment of bicycle and transit infrastructure, and they requested further information about how this design standard will be applied to this project and throughout the city.

The Commission looks forward to an additional concept submission for the southern segment of this project that responds to these comments. Please coordinate the next submission with the staff which, as always, is available to assist you.

Sincerely,

Thomas E. Luebke, FAIA
Secretary

Everett Lott, Interim Director
D.C. Department of Transportation
55 M Street, SE, Suite 400
Washington, DC 20003

cc: Mike Gifford, PKAK

Streetscape Design

Design Features

- Protected bike lanes provided with connections to the existing Columbia Rd NW bike lanes, R St NW bike lane and the future 20th St NW protected bike lanes
- Improved landscaping and sidewalk amenities

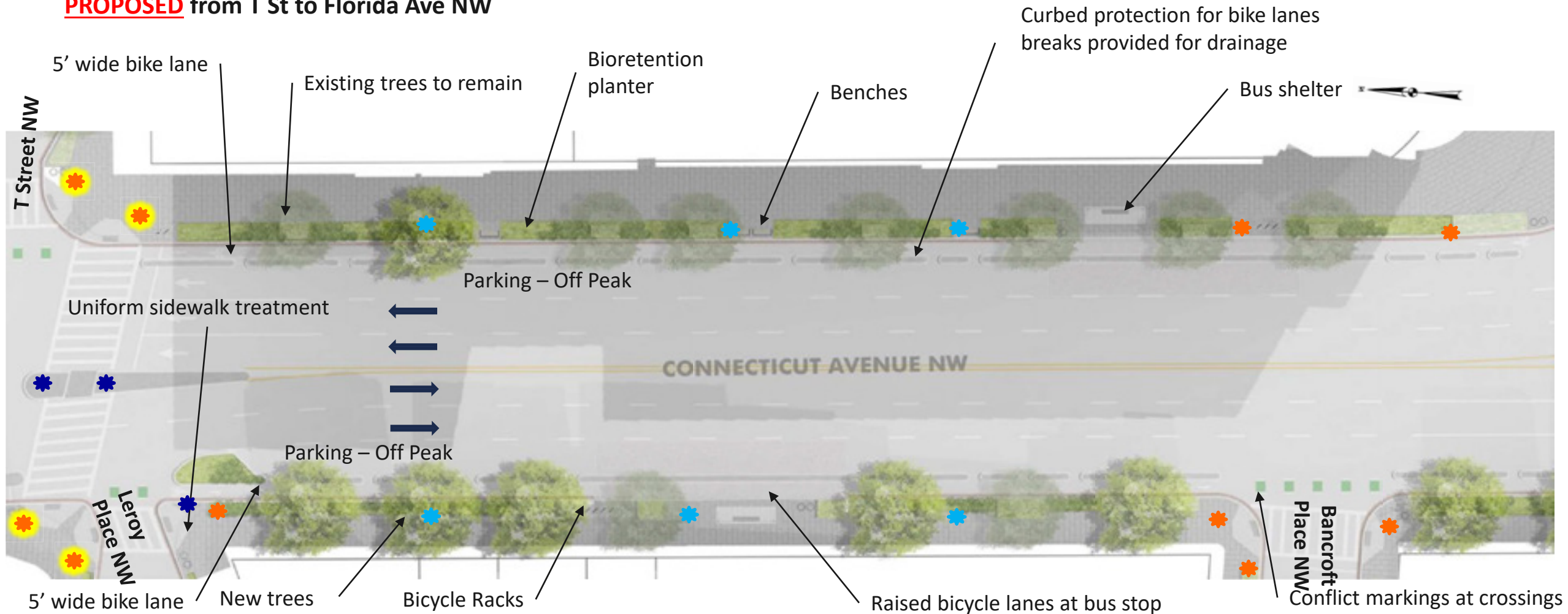


Streetscape Design

Example Block



PROPOSED from T St to Florida Ave NW



LIGHTING LEGEND

- 28.5' Pendant pole with teardrop luminaire
- Washington Globe – single luminaire
- Washington Globe – twin luminaire

- Standard Traffic Signal Pole
- Standard Pedestrian Signal Pole
- Indicates a lighting pole which also has signal equipment mounted to it

Streetscape Design

Example Block

PROPOSED from T St to Florida Ave NW



Plaza Design

Concept History

1960's



2014 - ANC



2016 - DDOT Feasibility Study



Several Visions over the Years

Design Illustrations have ranged
from Formal to Passive to Active
Uses on Plaza

Existing Conditions

Plaza

EXISTING

from Dupont Circle to
Q St NW



Existing Conditions

Plaza

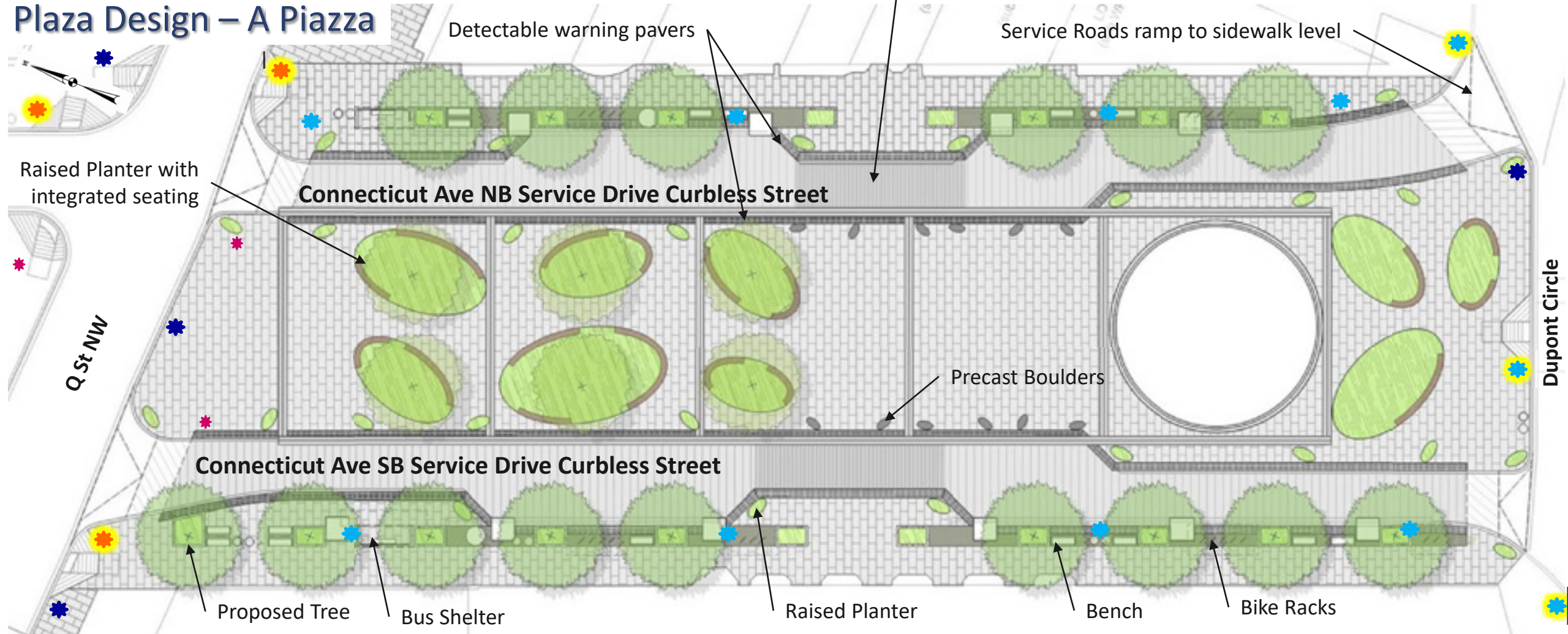
EXISTING

from Dupont Circle to
Q St NW



January 2021 Design

Plaza Design – A Piazza



LIGHTING LEGEND

- 28.5' Pendant pole with teardrop luminaire
- Washington Globe – single luminaire
- Washington Globe – twin luminaire

- Standard Traffic Signal Pole
- Standard Pedestrian Signal Pole
- Indicates a lighting pole which also has signal equipment mounted to it

CFA Comments from January 2021

Plaza Comments

- Encouraged continuous street tree placement along plaza
- Regular placement of the honey locusts in planters
- Supported oval plants and playfulness of varying sizes
- Recommended stone in place of precast concrete boulders or use of more regular and substantial design element to avoid future installation of supplemental barriers



U.S. COMMISSION OF FINE ARTS

ESTABLISHED BY CONGRESS 17 MAY 1910

401 STREET NW SUITE 511 WASHINGTON DC 20001-2726 202-644-1234 FAX 202-644-2746 WWW.CFA.DC

28 January 2021

Dear Mr. Lott:

In its public meeting of 21 January conducted by videoconference, the Commission of Fine Arts reviewed a concept design for proposed streetscape improvements along Connecticut Avenue, NW, between Dupont Circle and California Street. The Commission approved the concept for the northern portion of the project, from California Street to Q Street; for the southern portion of the project from Q Street to Dupont Circle, which includes a new plaza atop the open vehicular underpass, the Commission took no action and requested a further concept submission that responds to the following comments.

The Commission members expressed support for the creation of a pedestrian plaza to animate the block of Connecticut Avenue immediately north of Dupont Circle, but they raised numerous concerns about the proposed design. Commenting that the relationship of this block to Dupont Circle is critical to conceptualizing the design, they requested that the plan drawings include the open space, landscape, and fountain of Dupont Circle for reference. They encouraged the continuous placement of street trees without interruption at the pedestrian crosswalk; for the honey locust trees proposed in planters located within the plaza, they suggested that these be as regular as possible to reinforce the visual connection to the open space of Dupont Circle, possibly with additional trees at the north and south ends of the plaza. They acknowledged the advantages of the planters' oval shape in providing increased soil capacity within this narrow corridor, while expressing some support for the playfulness of the proposed configuration of various sizes and alignments, they also requested that the next submission include an alternative design with a more regular configuration of planters. Acknowledging the intent to protect pedestrians from vehicles, they recommended using natural stone instead of precast concrete for the curbside boulders, or using a more regular and substantial design element, they urged the development of a design that will not require the future addition of unsightly supplemental barriers.

In their support for the overall streetscape project, the Commission members observed that bicycle and bus lanes in Washington are often painted in bright colors that can be jarring within the urban context. They encouraged the D.C. Department of Transportation's citywide reconsideration of the standard design treatment of bicycle and transit infrastructure, and they requested further information about how this design standard will be applied to this project and throughout the city.

The Commission looks forward to an additional concept submission for the southern segment of this project that responds to these comments. Please coordinate the next submission with the staff which, as always, is available to assist you.

Sincerely,

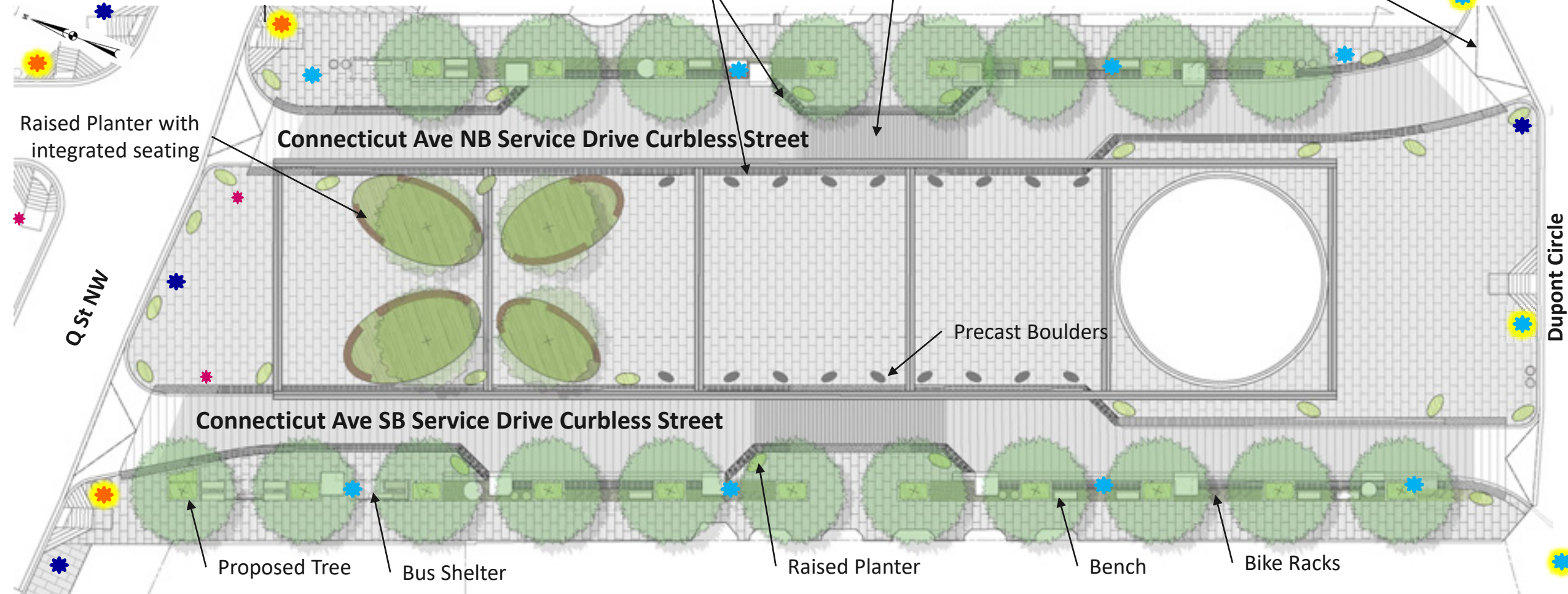
Thomas E. Luebke, FAIA
Secretary

Everett Lott, Interim Director
D.C. Department of Transportation
55 M Street, SE, Suite 400
Washington, DC 20003

cc: Mike Gifford, PKAK

November 2021 Design

Plaza Design – A Piazza



LIGHTING LEGEND

- 28.5' Pendant pole with teardrop luminaire
- Washington Globe – single luminaire
- Washington Globe – twin luminaire

- Standard Traffic Signal Pole
- Standard Pedestrian Signal Pole
- Indicates a lighting pole which also has signal equipment mounted to it

November 2021 Design

Plaza Design – Connection to Dupont Circle



November 2021 Design

Plaza Design – A Piazza



Goal:

- Flexibility

Considerations:

- Accessibility
- Safety
- Furnishings
- Programming,
Performances,
Markets, etc.
- Seasonal Use
- Visual lines

November 2021 Design

Plaza Design – Curbless Street Design



**Distinct separation
of vehicular lanes**

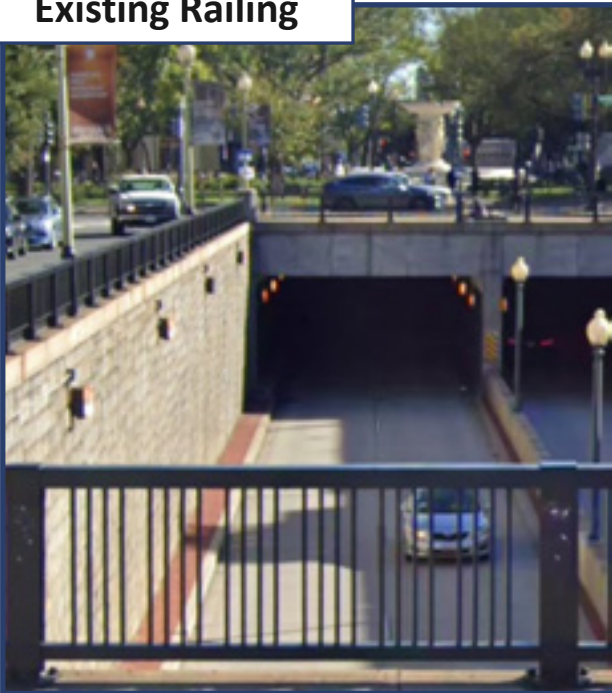
- Tactile pavers
- Vertical elements

Chicane

Opening Barrier

TL-4 Rated Barrier with Metal Pickets

Existing Railing



Proposed Railing

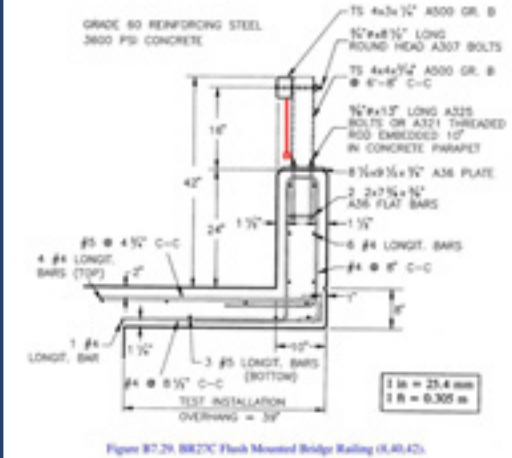
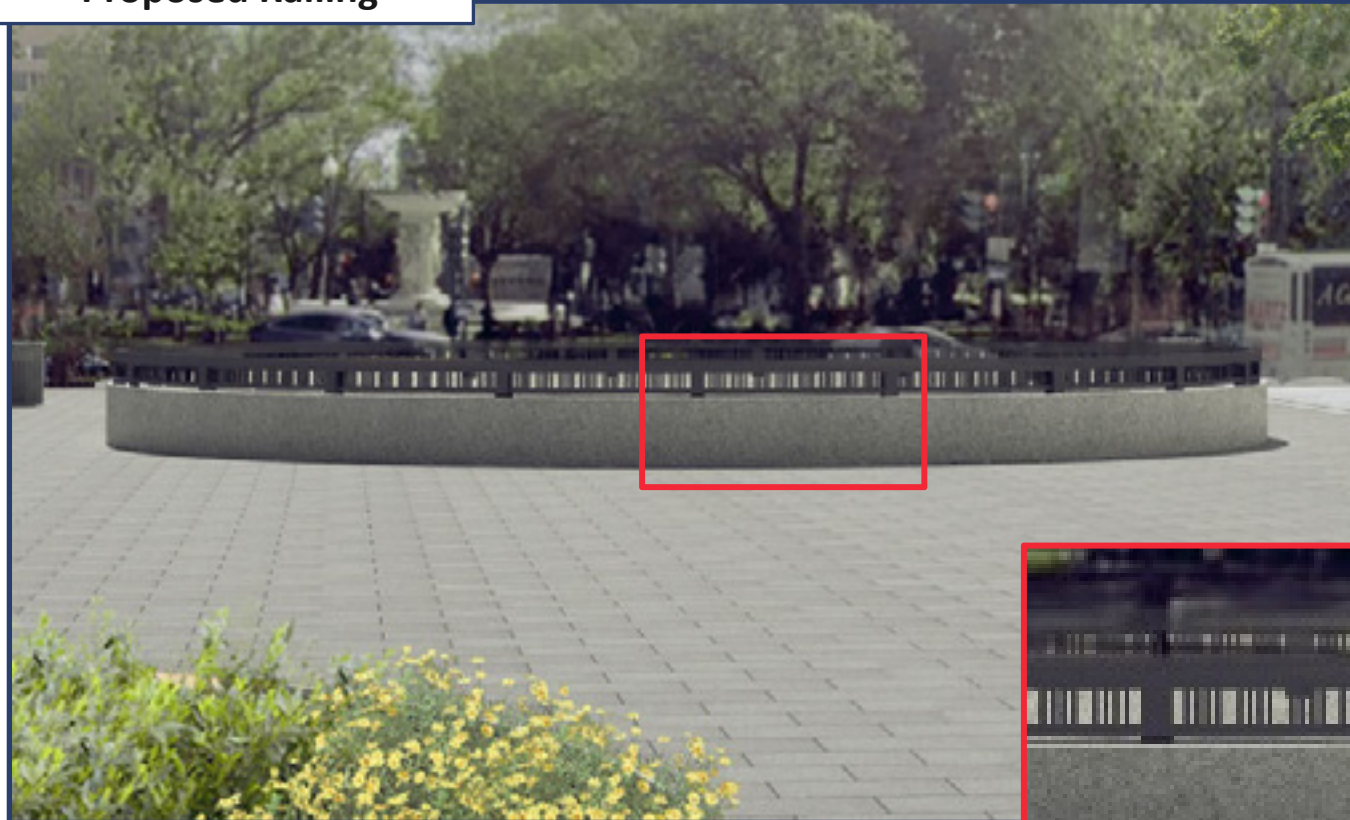
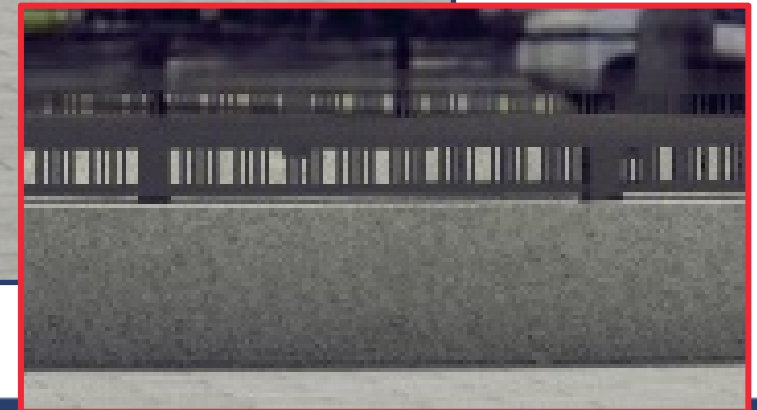


Figure B7.29. BR27C Thick Mounted Bridge Railing (LR442).



Schedule & Next Steps

Targeted Schedule:



Adjacent Projects:

- WMATA Escalator Canopy**
- 20th-21st Protected Bike Lanes**
- National Park Service Dupont Circle Rehabilitation**

**Not directly associated with this project

Thank you for your time. We would be happy to answer any questions.

For more information on the project, please visit our website or email us

- Project website: www.ctavestreetscapeplaza.com
- Email: CtAveStreetscape@gmail.com

Additional Slides and Background

- *Streetscape Elements*
- *Streetscape Design*
- *Plaza with Café seating*

Streetscape Design

Paving

Sidewalk & Plaza – Pressed Concrete Pavers



Furnishing Zone

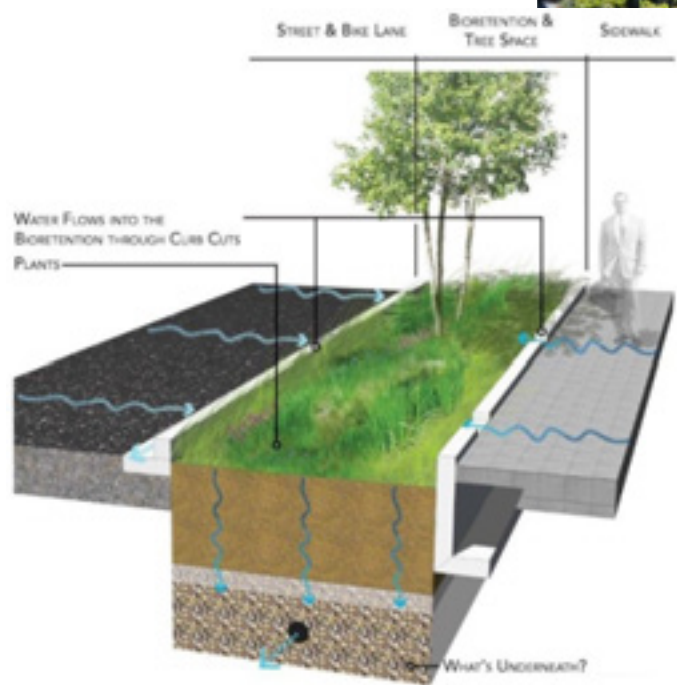


Streetscape Design

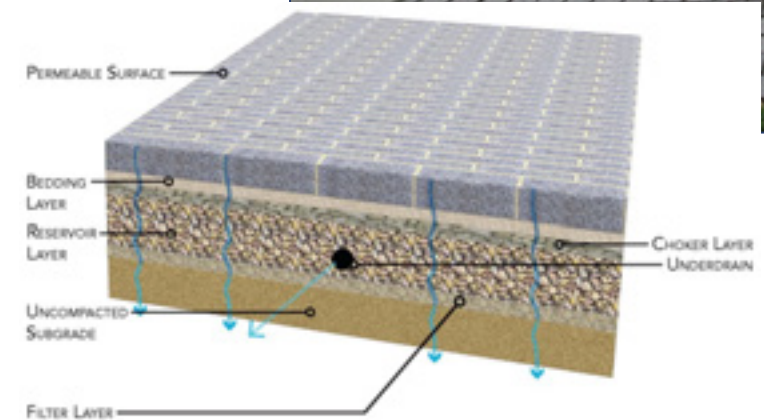
Stormwater Management



Bioretention Planters



Permeable Paving

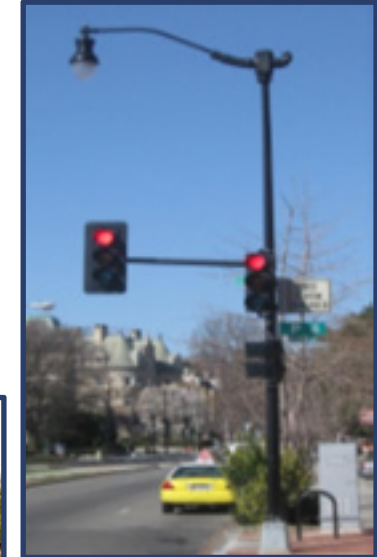
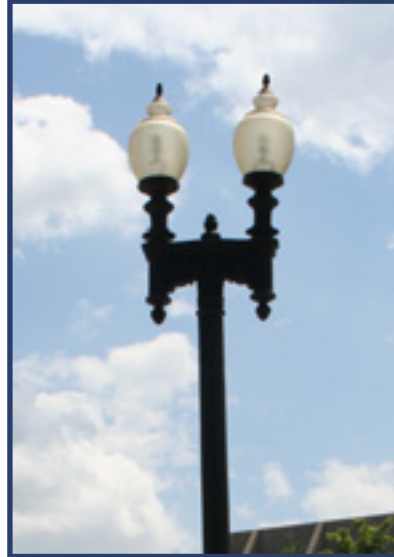


Streetscape Design

Furnishings

DDOT Standards

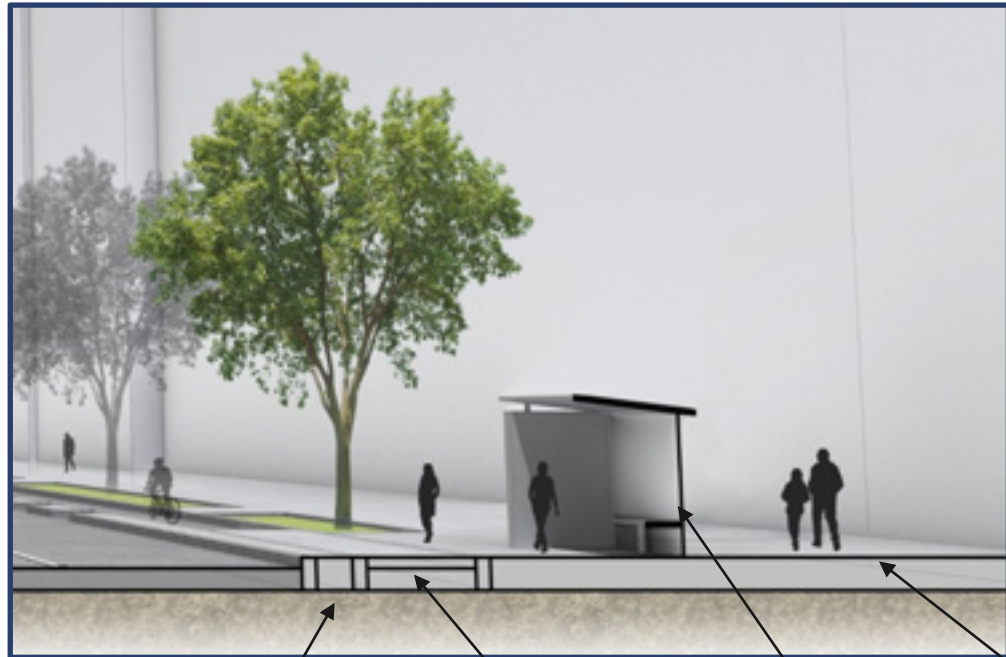
- Streetlights / Traffic Signals
- Benches
- Bike Racks
- Multi-space meters



Streetscape Design

Bus Stop – Bike Lane Interaction

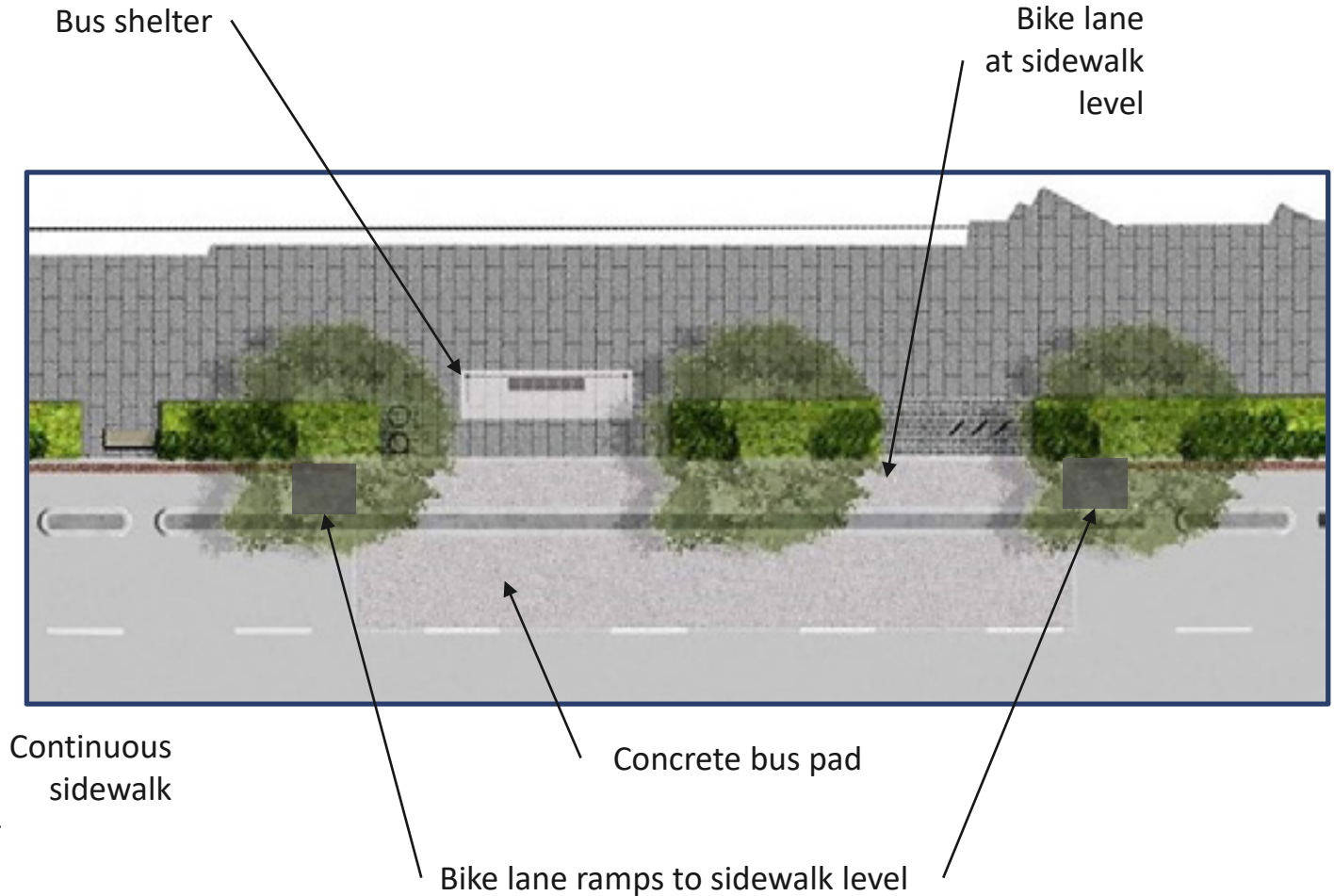
Example Section – Between T St and Florida Ave NW



Bike lane separator/
bus access point

Bike lane
at sidewalk
level

Bus shelter



Bus shelter

Bike lane
at sidewalk
level

Continuous
sidewalk

Concrete bus pad

Bike lane ramps to sidewalk level

Streetscape Design

Planting Schedule - Trees



Scarlet Oak

Quantity	Latin Name	Common Name	Size
4	Gleditsia triacanthos inermis 'Skycole'	Skyline Honeylocust	2.5" cal.
2	Magnolia grandiflora	Beauty Magnolia	2"-2.5" cal.
4	Platanus x acerifolia 'Bloodgood'	Bloodgood London Plane Tree	2.5" cal.
27	Quercus bicolor	Swamp White Oak	2.5" cal.
19	Quercus coccinea	Scarlet Oak	2.5" cal.



Swamp White Oak

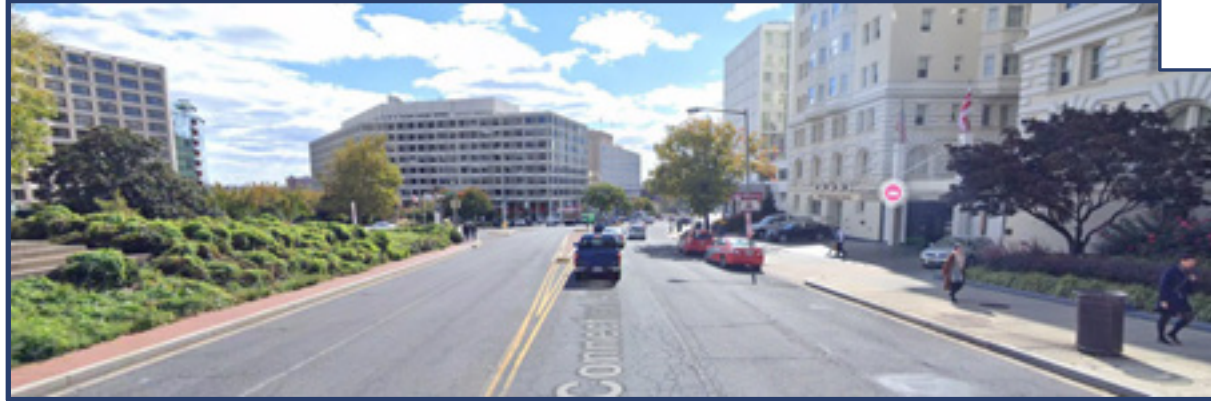


Skyline Honeylocust

Streetscape Design

Streetscape

EXISTING from Columbia Road to T Street NW



Connecticut Avenue NW
looking SB



Columbia Road NW
looking NB

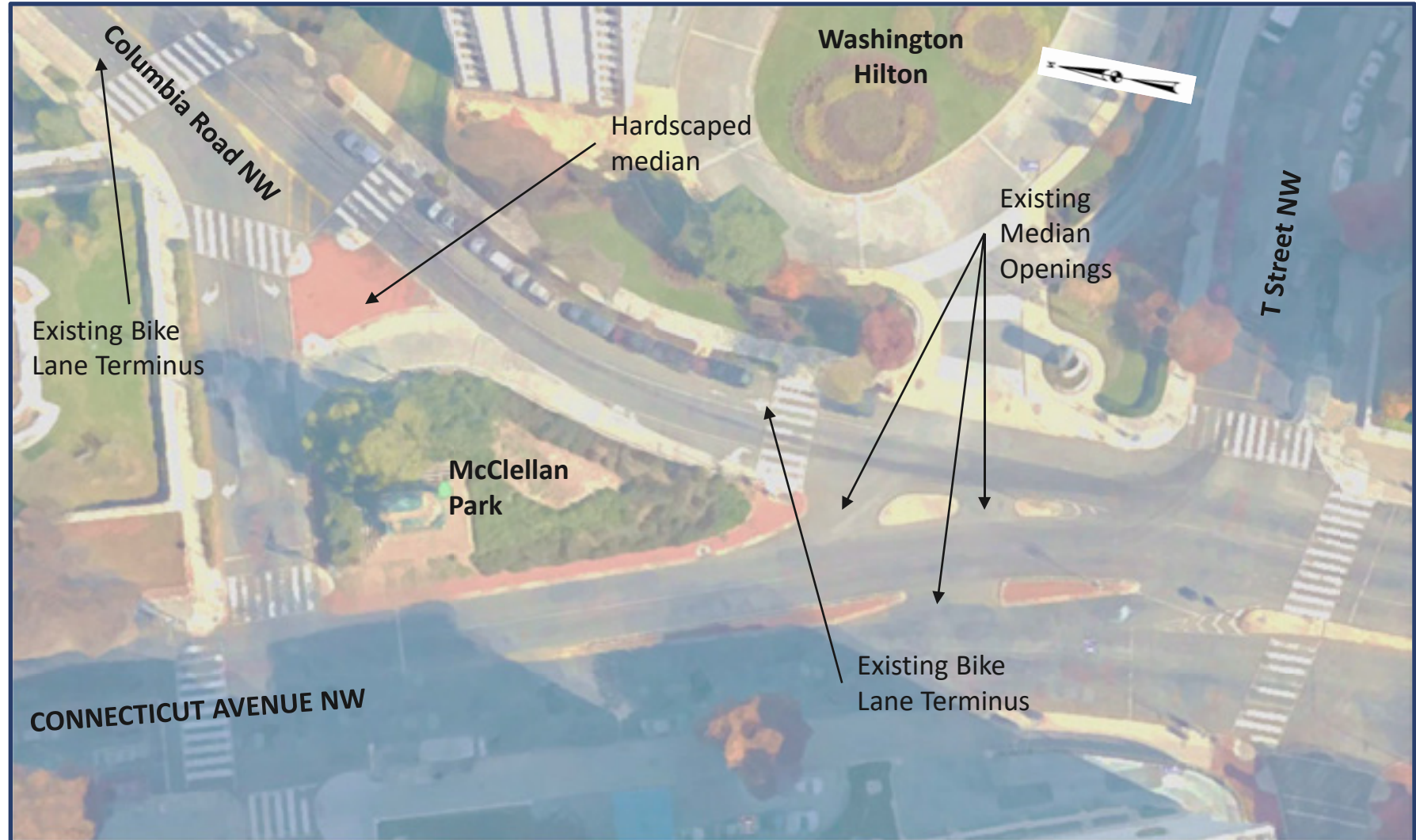
Streetscape Design

Streetscape

EXISTING

from Columbia Road to T St NW

- Bike lanes terminate before reaching Connecticut Ave
- Most medians are hardscaped; geometries do not promote perpendicular crossings
- Entrance to Hilton promotes unsafe turning movements



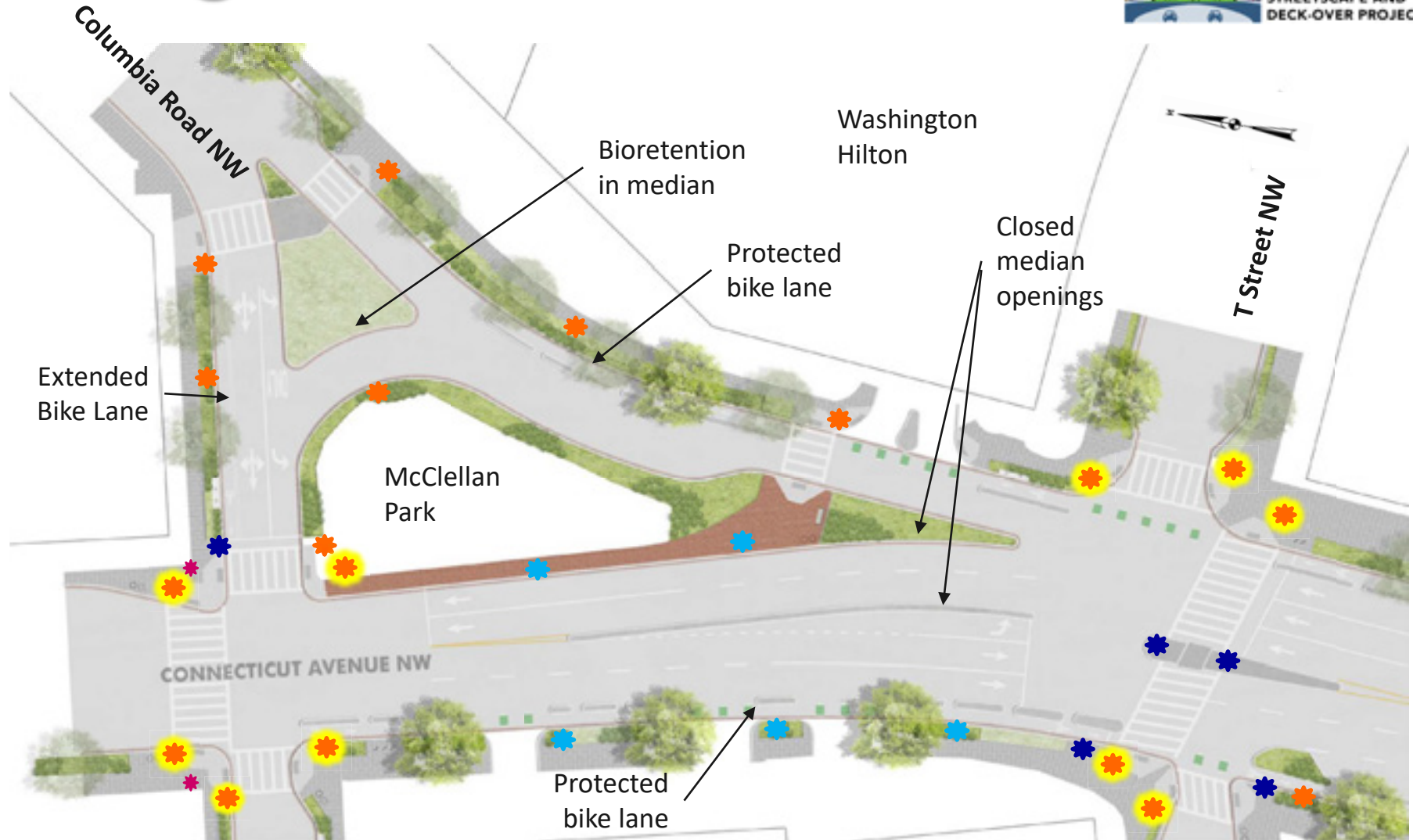
Streetscape Design

Streetscape

PROPOSED

from Columbia Road to T St NW

- Bike lanes connect to existing facilities on Columbia Road
- Median geometries and surface treatments improved
- Enhanced safety at Hilton entrance



LIGHTING LEGEND

- ★ 28.5' Pendant pole with teardrop luminaire
- ★ Washington Globe – single luminaire
- ★ Washington Globe – twin luminaire

- ★ Standard Traffic Signal Pole
- ★ Standard Pedestrian Signal Pole
- ★ Indicates a lighting pole which also has signal equipment mounted to it

Streetscape Design

Streetscape

EXISTING from T St to Florida Ave NW

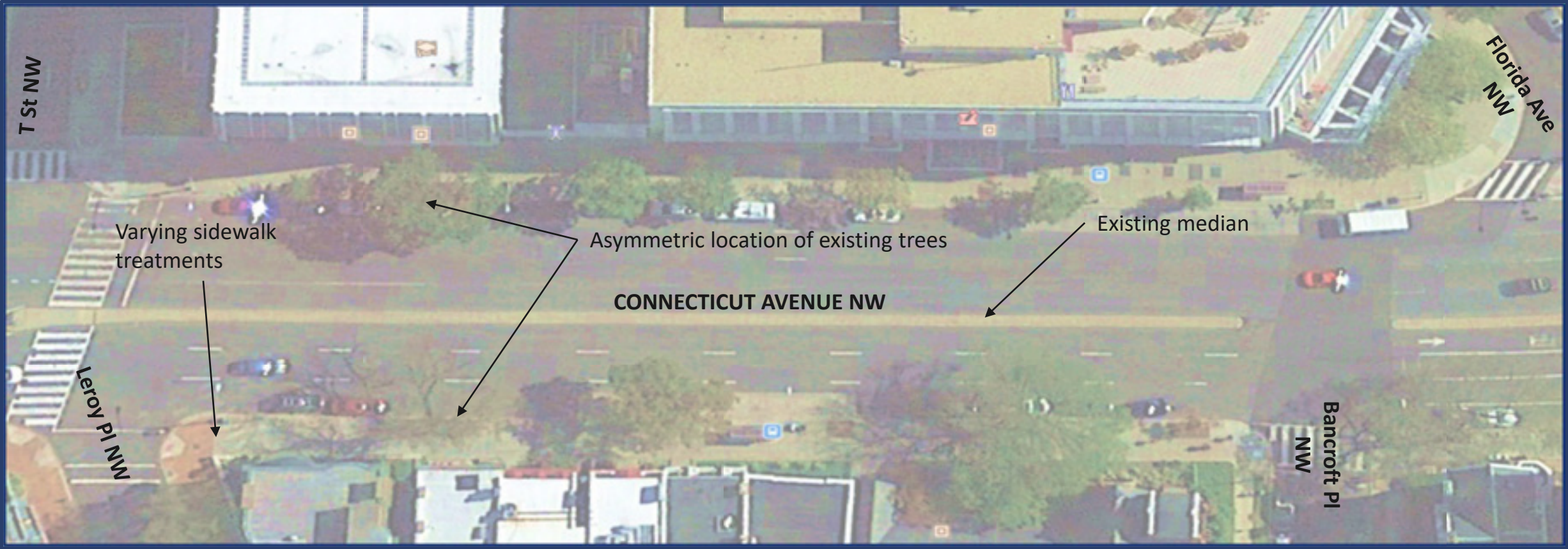


Connecticut Avenue NW
looking SB

Streetscape Design

Streetscape

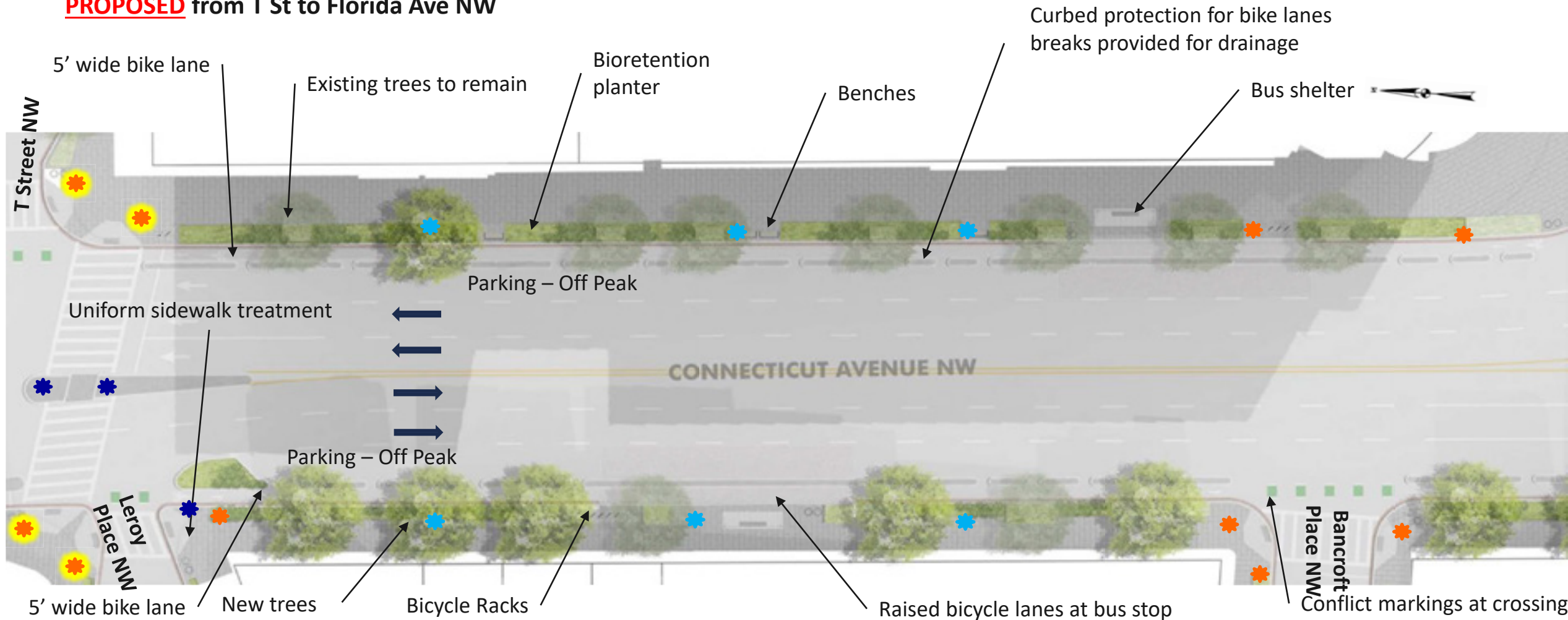
EXISTING from T St to Florida Ave NW



Streetscape Design



PROPOSED from T St to Florida Ave NW



LIGHTING LEGEND

- 28.5' Pendant pole with teardrop luminaire
- Washington Globe – single luminaire
- Washington Globe – twin luminaire

- Standard Traffic Signal Pole
- Standard Pedestrian Signal Pole
- Indicates a lighting pole which also has signal equipment mounted to it

Streetscape Design

PROPOSED from T St to Florida Ave NW



Streetscape Design

Streetscape

EXISTING at Florida Ave NW



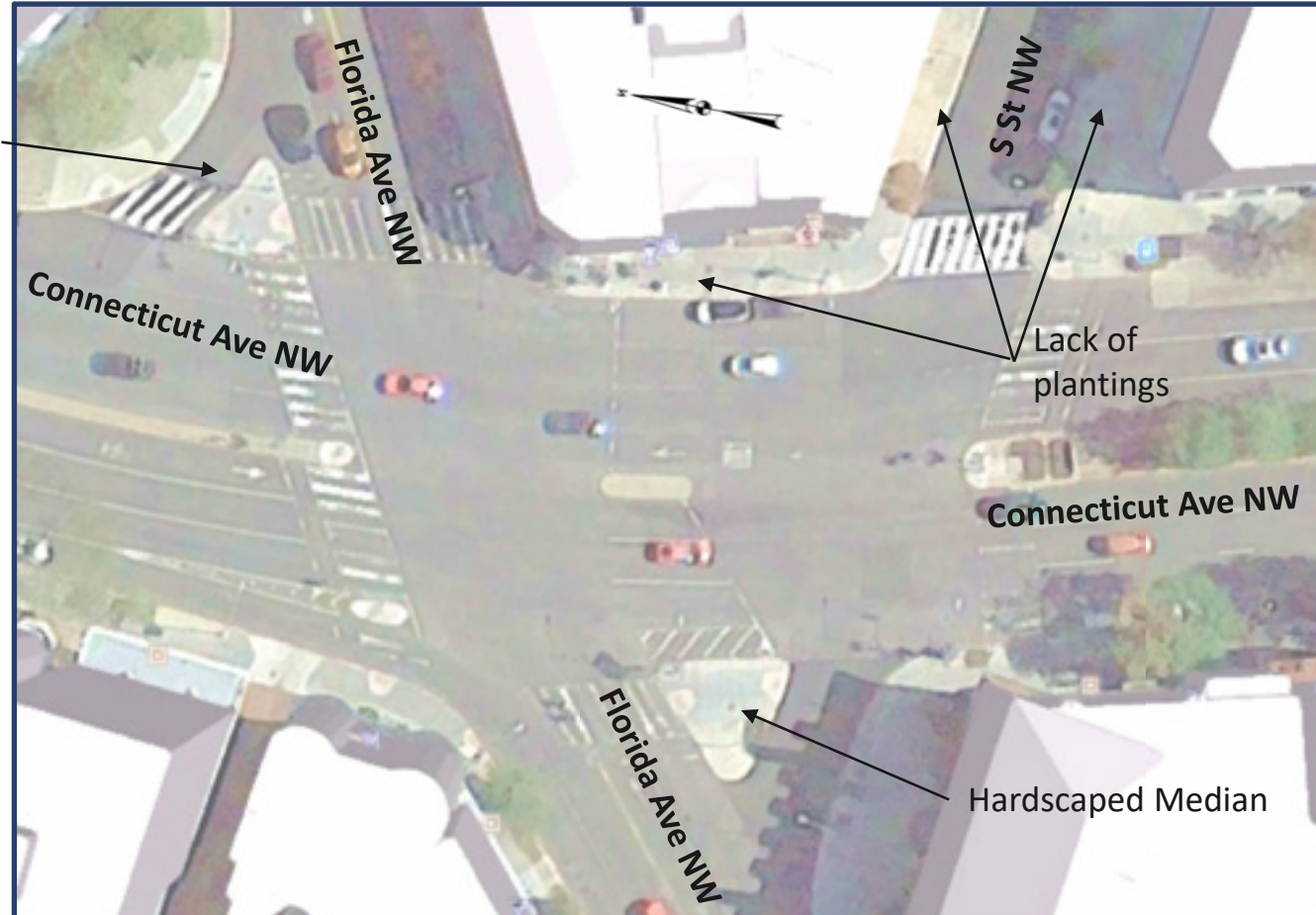
Connecticut Avenue NW
looking SB

Streetscape Design

Streetscape

EXISTING at Florida Ave NW

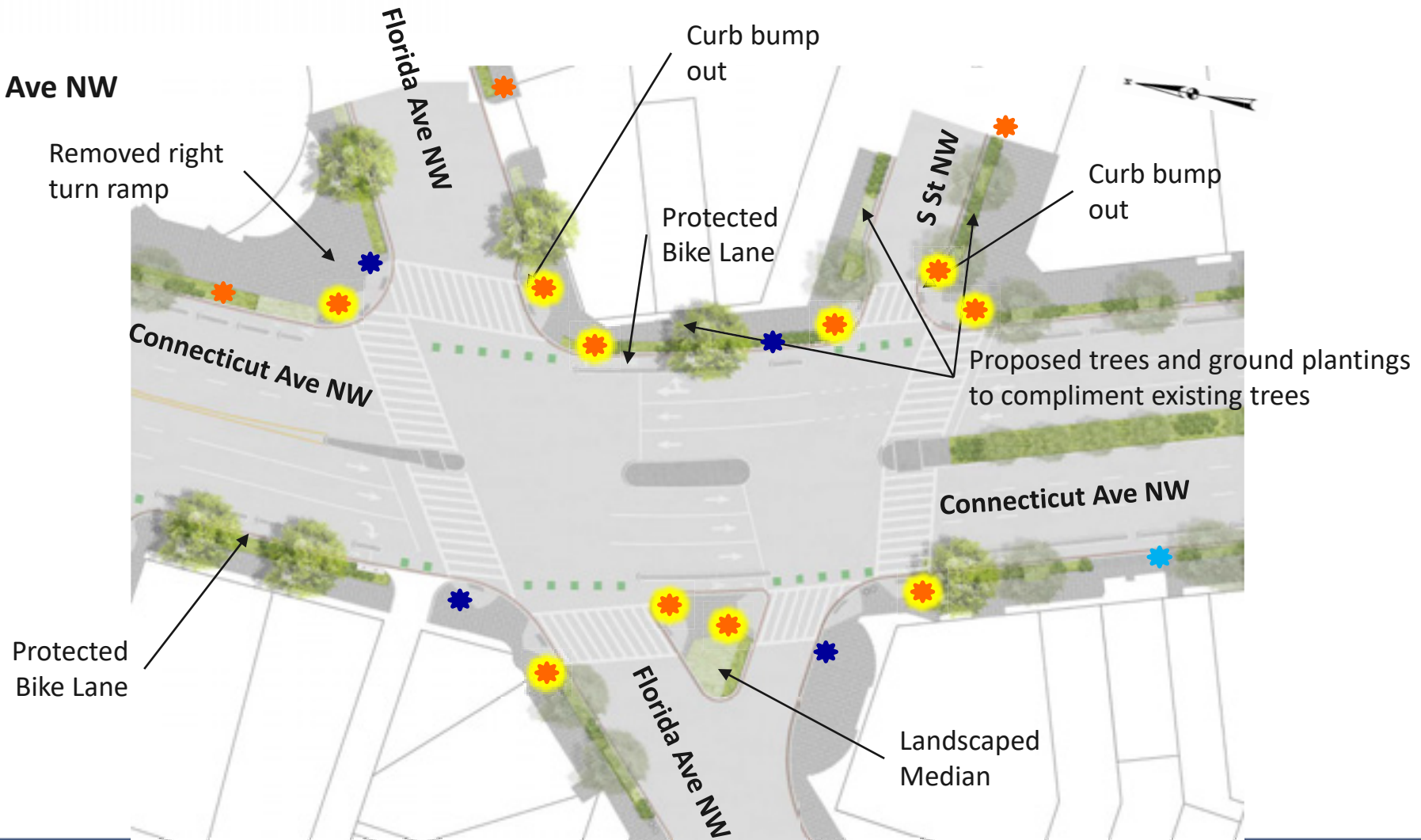
Right Turn Slip Ramp



Streetscape Design

Streetscape

PROPOSED at Florida Ave NW



LIGHTING LEGEND

- Orange star: 28.5' Pendant pole with teardrop luminaire
- Purple star: Washington Globe – single luminaire
- Blue star: Washington Globe – twin luminaire

- Blue star: Standard Traffic Signal Pole
- Pink star: Standard Pedestrian Signal Pole
- Yellow star: Indicates a lighting pole which also has signal equipment mounted to it

Streetscape Design

Streetscape

EXISTING from S St to R St NW

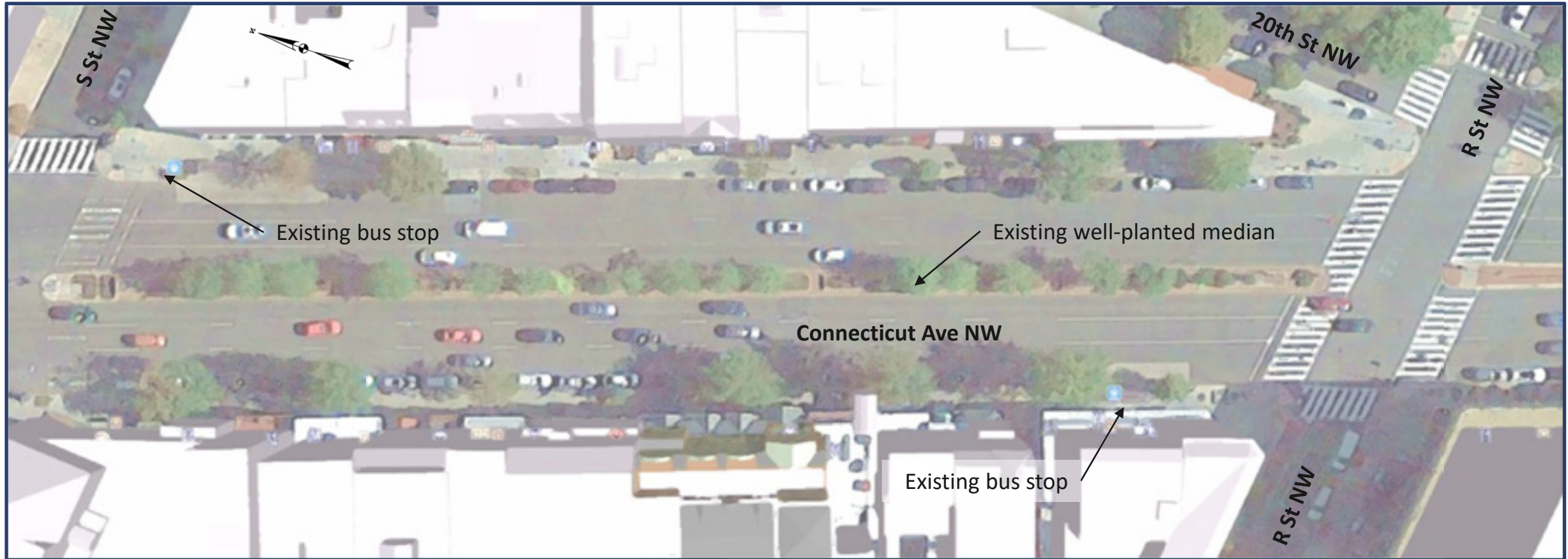


Connecticut Avenue NW
looking SB

Streetscape Design

Streetscape

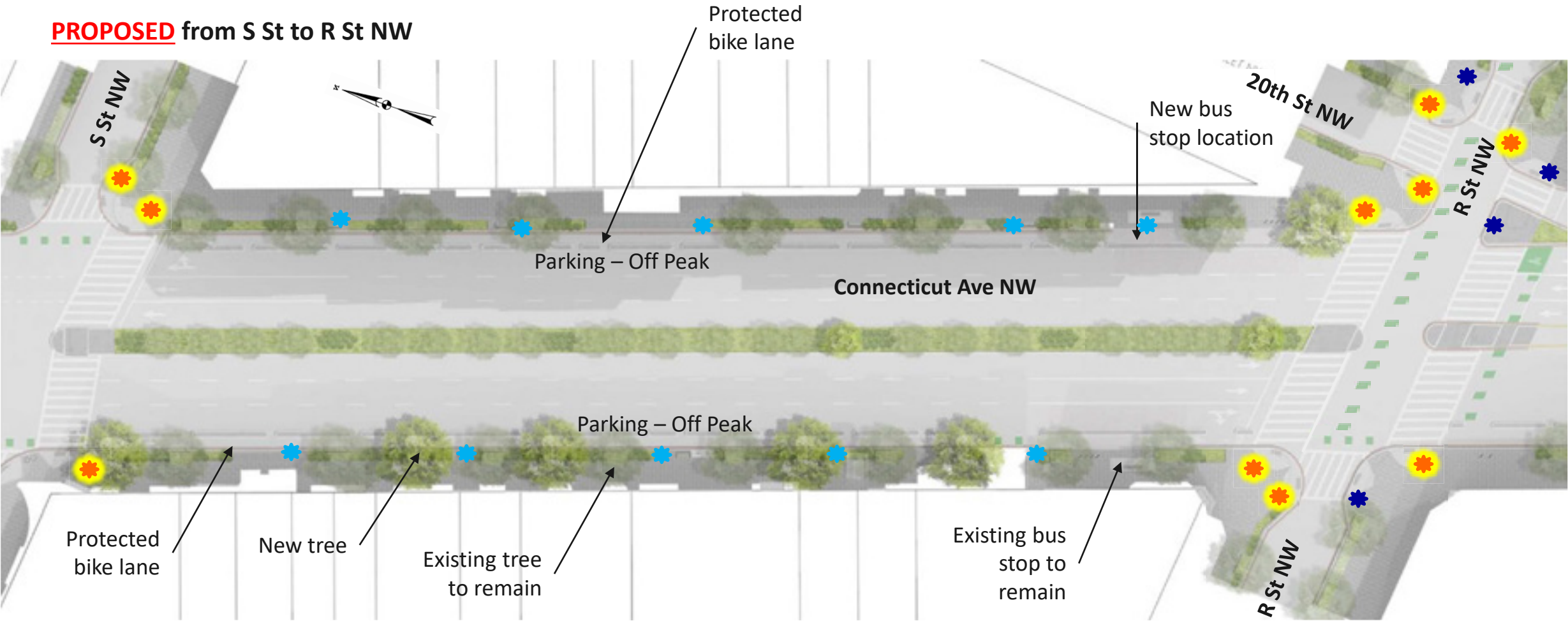
EXISTING from S St to R St NW



Streetscape Design

Streetscape

PROPOSED from S St to R St NW



LIGHTING LEGEND

- 28.5' Pendant pole with teardrop luminaire
- Washington Globe – single luminaire
- Washington Globe – twin luminaire

- Standard Traffic Signal Pole
- Standard Pedestrian Signal Pole
- Indicates a lighting pole which also has signal equipment mounted to it

Streetscape Design

Streetscape

PROPOSED from S St to R St NW



Streetscape Design

Streetscape

EXISTING from R St to 20th St NW



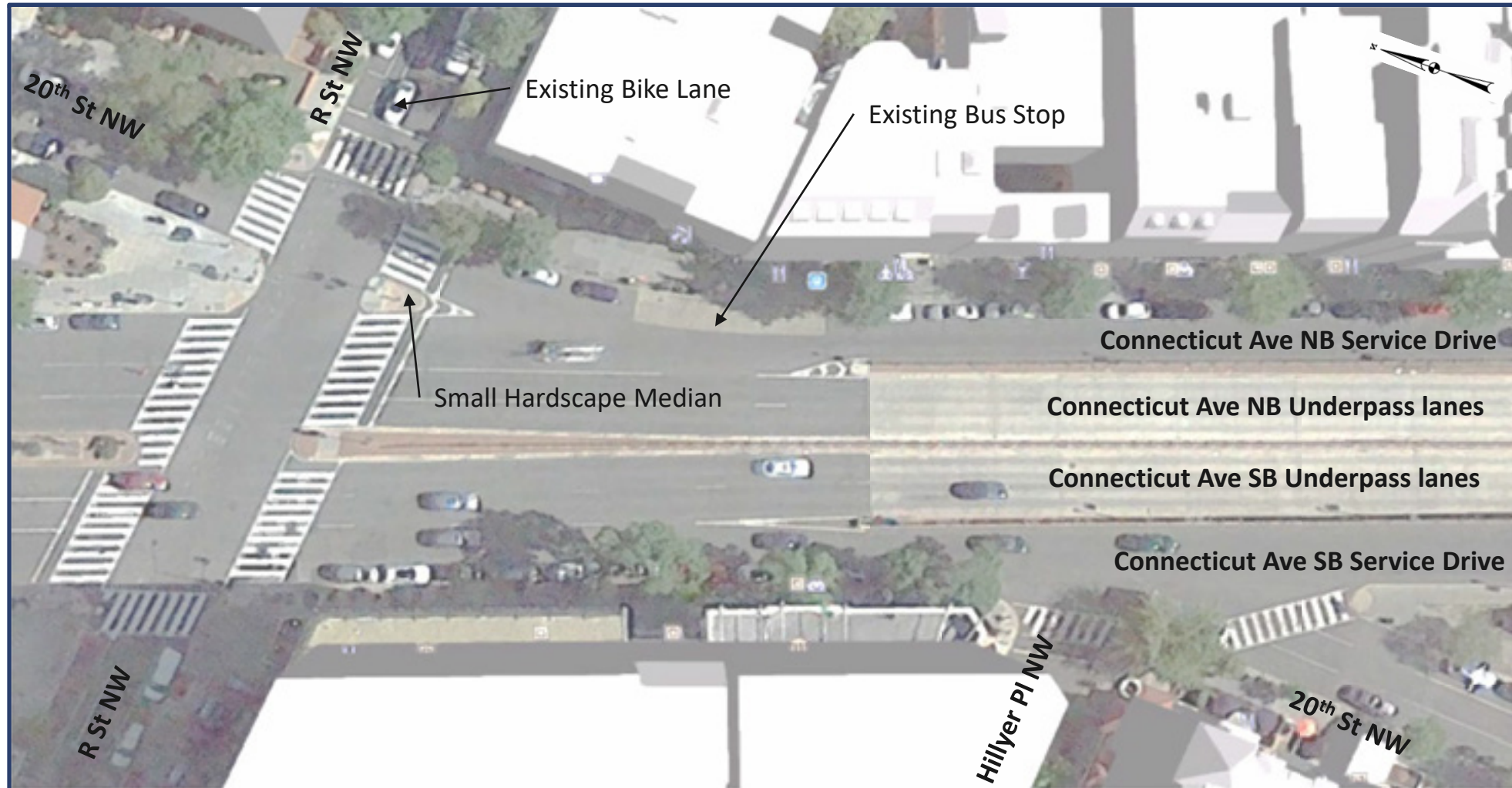
Connecticut Avenue NW
looking SB

Streetscape Design

Streetscape

EXISTING

from R St to 20th St NW



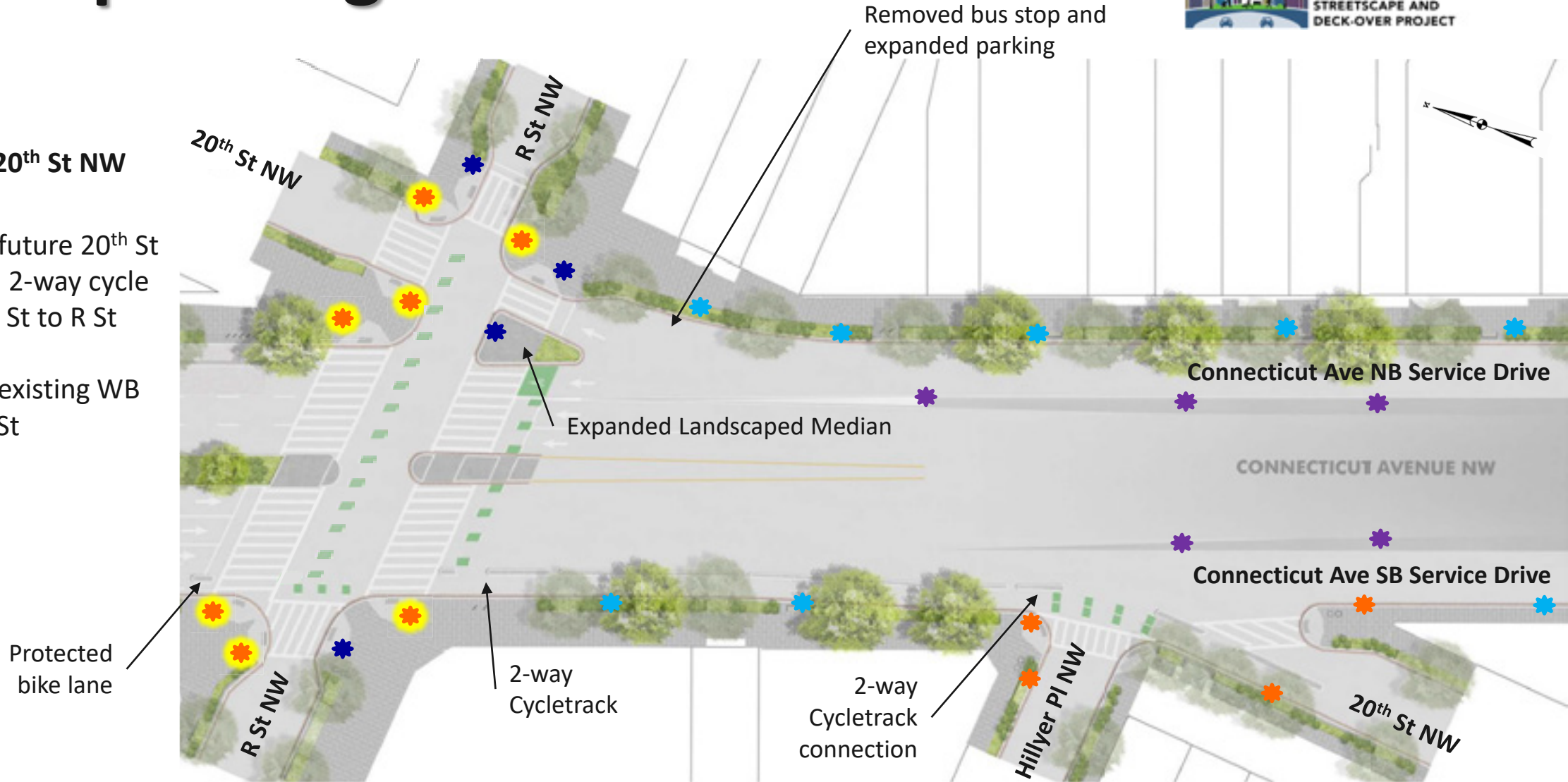
Streetscape Design

Streetscape

PROPOSED

from R St to 20th St NW

- Connection to future 20th St bike lanes via a 2-way cycle track from 20th St to R St
- Connection to existing WB bike lane on R St



LIGHTING LEGEND

- ★ 28.5' Pendant pole with teardrop luminaire
- ★ Washington Globe – single luminaire
- ★ Washington Globe – twin luminaire

- ★ Standard Traffic Signal Pole
- ★ Standard Pedestrian Signal Pole
- ★ Indicates a lighting pole which also has signal equipment mounted to it

Streetscape Design

Streetscape

EXISTING

from 20th St to Q St NW

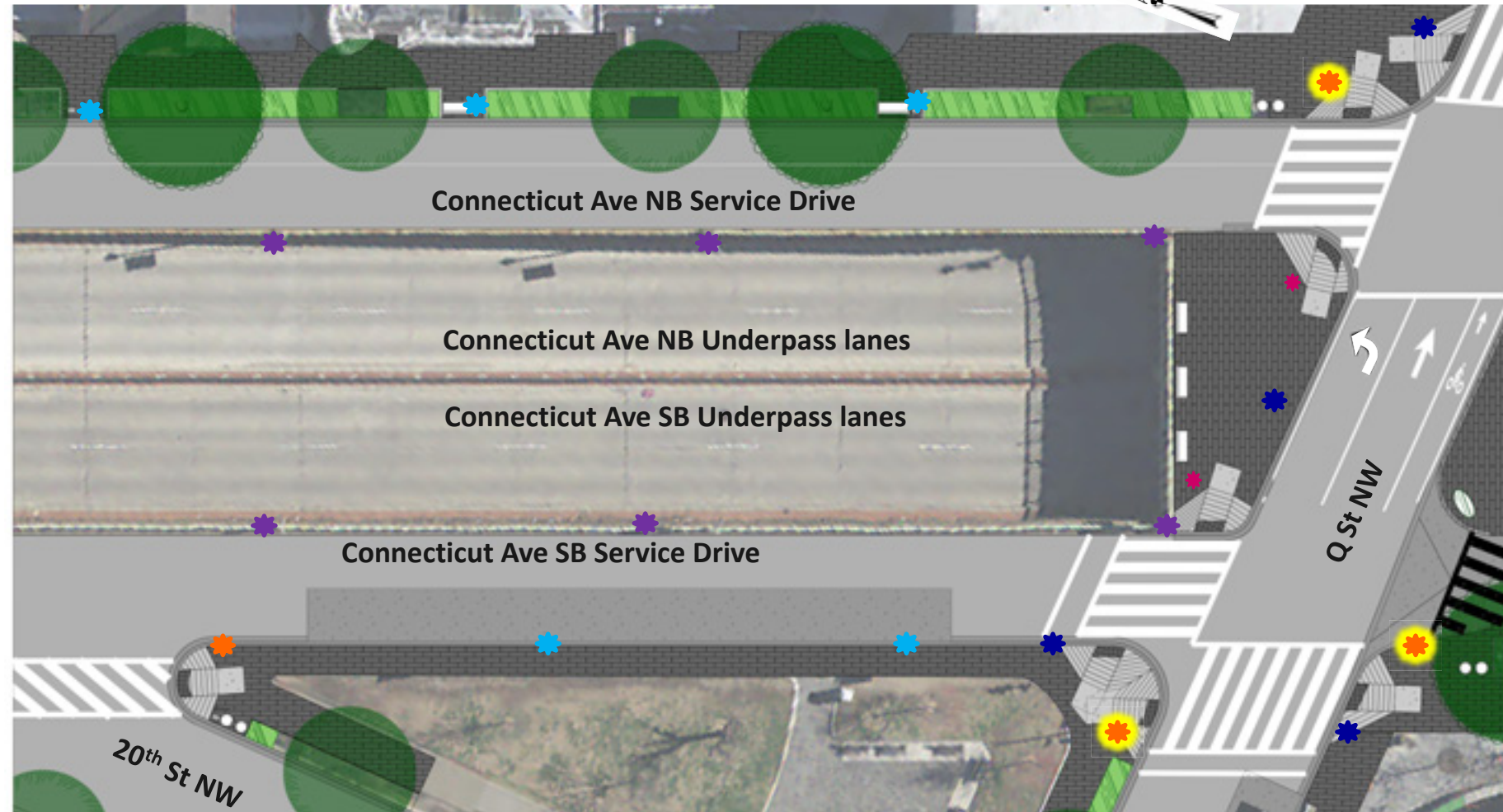


Streetscape Design

Streetscape

PROPOSED

from 20th St to Q St NW



LIGHTING LEGEND

- ★ 28.5' Pendant pole with teardrop luminaire
- ★ Washington Globe – single luminaire
- ★ Washington Globe – twin luminaire

- ★ Standard Traffic Signal Pole
- ★ Standard Pedestrian Signal Pole
- ★ Indicates a lighting pole which also has signal equipment mounted to it

Plaza Design Examples

Plaza Design – A Piazza

Curbless Street Examples



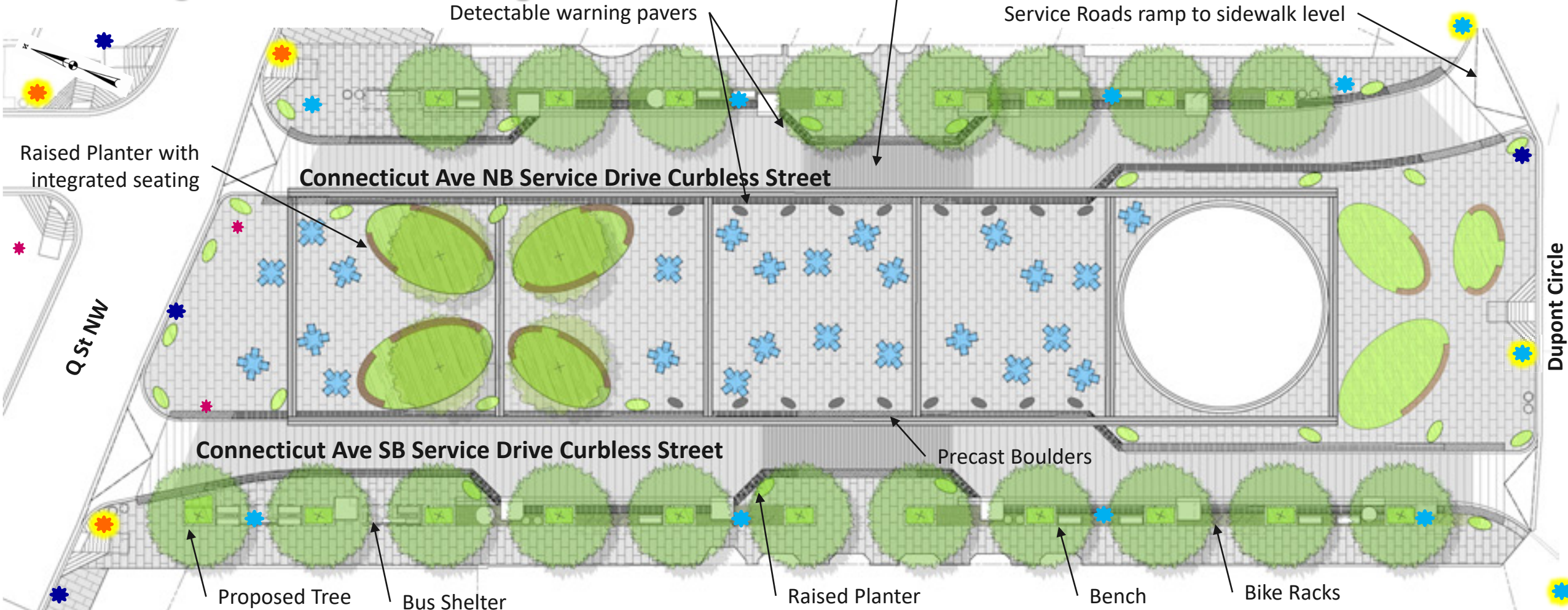
West Georgia Street, Indianapolis, IN



Bell Street, Seattle, WA

November 2021 Design

Plaza Design – With Café Seating



LIGHTING LEGEND

- 28.5' Pendant pole with teardrop luminaire
- Washington Globe – single luminaire
- Washington Globe – twin luminaire

- Standard Traffic Signal Pole
- Standard Pedestrian Signal Pole
- Indicates a lighting pole which also has signal equipment mounted to it